

STREET BLOCK PLAN FOR BEACH ROAD, TAN QUEE LAN STREET, NORTH BRIDGE ROAD AND SEAH STREET (DOWNTOWN CORE PLANNING AREA) (URA/ CUD PLAN RELEASE 1/2025E)

The purpose of this street block plan is to guide additions & alterations (A&As) and redevelopment proposals within the Beach Road, Tan Quee Lan Street, North Bridge Road, Middle Road and Seah Street street blocks to ensure that developments have a strong street edge and contribute to the planned comprehensive continuous covered walkway network for comfortable pedestrian movement at street level.

The prescribed planning parameters and urban design guidelines for the street block plan are set out below.

| PARAMETERS | | REQUIREMENTS | |
|------------|-------------------------|--|---|
| | | Conserved Building | Other Envelope Control Sites |
| 1 | Land Use* | Commercial use for developments fronting Beach Road, North Bridge Road and Middle Road; and Commercial & Residential use for developments fronting Seah Street. Under this zoning, the Gross Floor Area (GFA) allowable for Commercial use is not to exceed 40% of the total allowable GFA. | |
| 2 | Gross Plot Ratio | Maximum 4.2 | |
| 3 | Conservation Guidelines | All relevant conservation guidelines apply. Please refer to Part 2: Planning Parameters and Restoration Guidelines, section 2.3: Secondary Settlements at https://www.ura.gov.sg/conservation/conservation-areas/secondary-settlements | N.A. |
| 4 | Building Height | The main building must be conserved. The rear portion of the site can be built up to: Maximum 6 storeys for developments fronting North Bridge Road. For 496 and 516 North Bridge Road: | Maximum 6 storeys for developments fronting Beach Road, North Bridge Road and Middle Road; and Maximum 5 storeys for developments fronting Seah Street and Purvis Street, excluding 29-38 Seah Street, 37-41 Beach Road, and 47 Beach Road, which are subject to detailed envelope control |

| PARAMETERS | | REQUIREMENTS | |
|------------|---|---|--|
| | | Conserved Building | Other Envelope Control Sites |
| | | <p>The 5th and 6th storeys are subject to a maximum floor-to-floor height of 3.5m, measured from the roof of the main conserved building, to reduce the impact of the rear extension on the lower-rise conserved shophouses along North Bridge Road and Liang Seah Street. (See Annexure 1); and</p> <p>Maximum 5 storeys for developments fronting Seah Street, Purvis Street and Liang Seah Street.</p> <p>See Appendix 1 for Building Form Plan.</p> | <p>guidelines.</p> <p>See Appendix 1 for Building Form Plan.</p> |
| 5 | Building Form | Party wall development. Developments are to be built up to the common boundaries with a party wall to the full height of the development. Any party wall that is exposed as an external wall must not have any openings. | |
| 6 | Building Setback / Building Edge [#] | <p>The conserved façade is to be retained and restored according to specific restoration guidelines for each building.</p> <p>You can request for the guidelines via https://eservice.ura.gov.sg/facadeWeb/.</p> | Developments are to be built up to about the lines of Road Reserve along Beach Road, North Bridge Road, Middle Road, Seah Street, Purvis Street, Liang Seah Street and Tan Quee Lan Street up to the full height of the development, as indicated in Appendix 2, unless otherwise stated |
| 7 | Covered Walkway | <p>A continuous covered walkway is to be provided and abut the lines of Road Reserve.</p> <p>Where the covered walkways of adjacent developments are not aligned, a covered linkway is to be constructed and maintained by the owner to bridge connectivity gaps and provide shelter in between the covered walkways.</p> <p>The Owner/Developer shall obtain a Temporary Occupation License (TOL) from the Singapore Land Authority (SLA) for use of the State land. The TOL may be granted on such terms and conditions and subject to the payment of such charges and fees as the SLA may determine.</p> | |

| PARAMETERS | | REQUIREMENTS | |
|------------|--|--|--|
| | | Conserved Building | Other Envelope Control Sites |
| | | <p>The covered walkway must be restored in accordance with the relevant conservation guidelines.</p> | <p>A minimum 3.6m wide (and 3.0m clear) continuous covered walkway is to be provided to abut the lines of Road Reserve along North Bridge Road, Beach Road and Middle Road.</p> <p>A minimum 3.0m wide (and 2.4m clear) continuous covered walkway is to be provided to abut the lines of Road Reserve along Seah Street, Purvis Street, Liang Seah Street and Tan Quee Lan Street.</p> <p>The covered walkways are:</p> <ul style="list-style-type: none"> a) To have a maximum external soffit height of 3.6m measured from the covered walkway level. Higher soffit heights can be considered, subject to the provision of drop-panels or the width of the walkway being increased to match the higher height. This is to ensure adequate weather protection for pedestrians in the event of inclement weather; b) To abut, open out onto and match the level of the open walkways within the adjacent Road Reserves and the covered walkways of the adjacent developments; c) To be at a constant level throughout the entire length. Where there are differences in level with the adjacent open walkways or the covered walkways of abutting developments, ramps must be built to mediate the differences. Steps are only allowed for steep gradients where ramps are not possible; and |

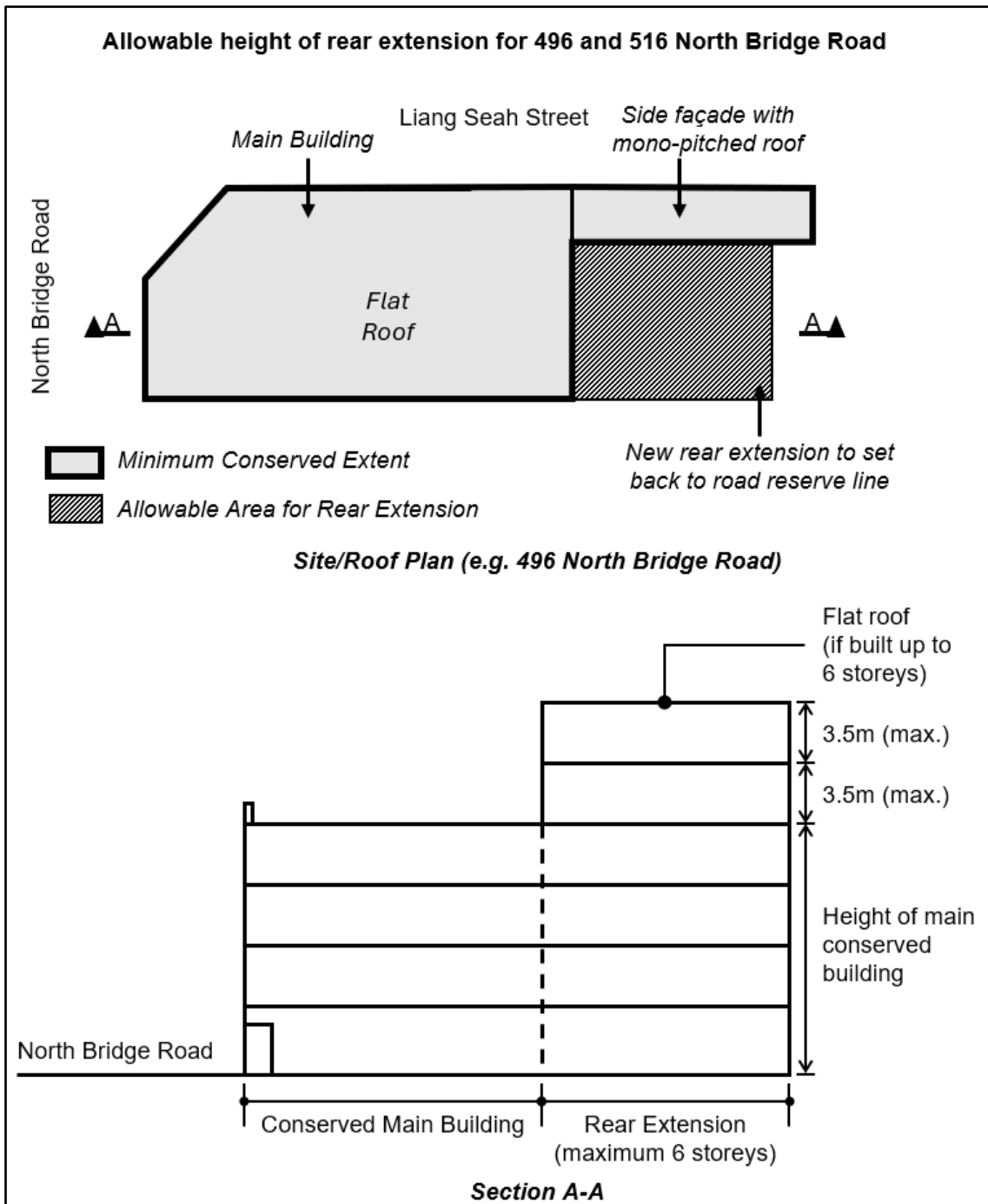
| PARAMETERS | | REQUIREMENTS | |
|------------|---------------|---|---|
| | | Conserved Building | Other Envelope Control Sites |
| | | | <p>d) Where there is a level difference between the covered walkway and the 1st storey of the development, ramps or steps between the two levels must be built within the 1st storey level of the development and not along the covered walkway.</p> <p>A colonnaded covered walkway is to be provided to maintain the continuity of the streetscape along Purvis Street and Seah Street.</p> |
| 8 | Roofscape | The roofscape must be restored in accordance with the relevant conservation guidelines. | <p>Roof areas are to be well-designed and attractive when viewed from the surrounding developments.</p> <p>For flat roofs, roof parapet walls may be built to a maximum height of 1m. However, architectural features on such walls may exceed this limit and will be evaluated and approved on a case-by-case basis.</p> |
| 9 | Service Areas | <p>All service areas, mechanical and electrical (M&E) equipment, water tanks, car parking lots, etc., are to be located within, and be fully integrated into, the building envelope and visually well-screened <u>from the top and on all sides</u>.</p> <p>Relevant Guideline: Screening of Mechanical and Electrical Services and Car Parks (URA/PB/2022/01-CUDG)</p> | |

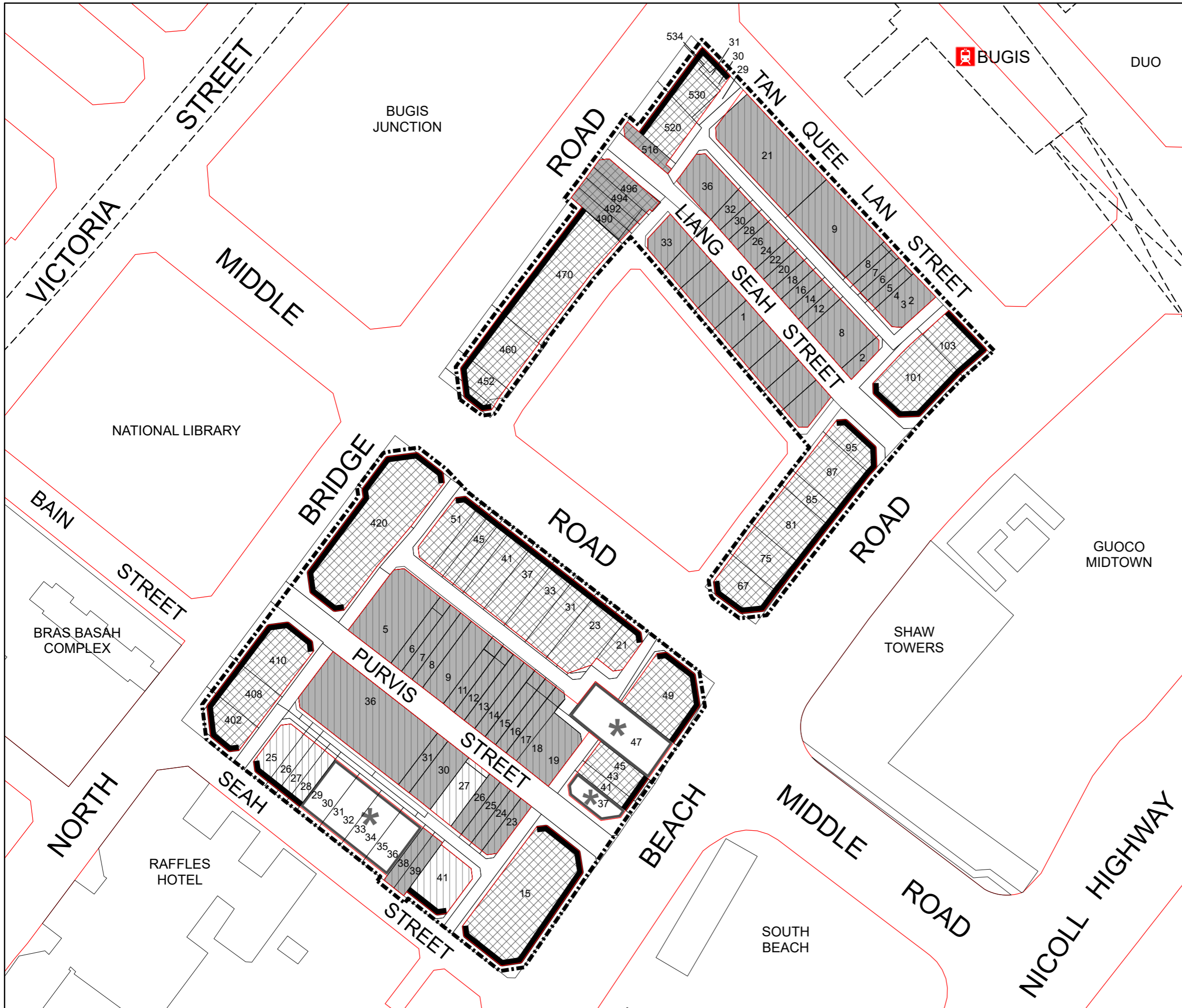
| PARAMETERS | | REQUIREMENTS | |
|------------|------------------|---|------------------------------|
| | | Conserved Building | Other Envelope Control Sites |
| 10 | Car Parking | <p>To comply with LTA's requirements, sufficient car parking spaces are to be provided within the site boundary of the development to meet the needs of the site's proposed uses.</p> <p>Provision of car parks or payment of car parks deficiency charge for a conserved building or any part thereof that is conserved is waived if the conservation guidelines are fully complied with and the conservation works are completed in accordance with the approved plans.</p> <p>The requirement for provision of carparking spaces under the Parking Places (Provision of Parking Places and Parking Spaces) Rules and any statutory modifications or re-enactments thereof for the time being in force shall be complied with in full for the new extension(s).</p> | |
| 11 | Vehicular Access | All vehicular access is to be taken from the safeguarded rear service roads. | |

* Please refer to the latest Master Plan land use zoning. Where there is any discrepancy, the Master Plan land use zoning shall prevail.

The plot for road widening is required by LTA to be vested free of charge in the State with vacant possession and free from encumbrances prior to the issue of CSC for the development.

DIAGRAM SHOWING ALLOWABLE HEIGHT OF REAR EXTENSION FOR 496 AND 516 NORTH BRIDGE ROAD





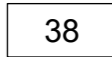








**URA/CUD PLAN
RELEASE 1/2025E**



**STREET BLOCK PLAN FOR
BEACH ROAD, TAN QUEE LAN
STREET, NORTH BRIDGE ROAD
AND SEAH STREET
(DOWNTOWN CORE
PLANNING AREA)**

BUILDING FORM PLAN

LEGEND

-  Boundary of Street Block Plan
-  Lines of Road Reserve
-  House Number
-  Developments to be built up to the Lines of Road Reserve
-  Conserved Buildings - Maximum Building Height 6 Storeys for the Rear Development
-  Conserved Buildings - Maximum Building Height 5 Storeys for the Rear Development
-  Site subject to Envelope Control Guidelines - Maximum Building Height 6 Storeys
-  Site subject to Envelope Control Guidelines - Maximum Building Height 5 Storeys
-  Site subject to Detailed Envelope Control Guidelines

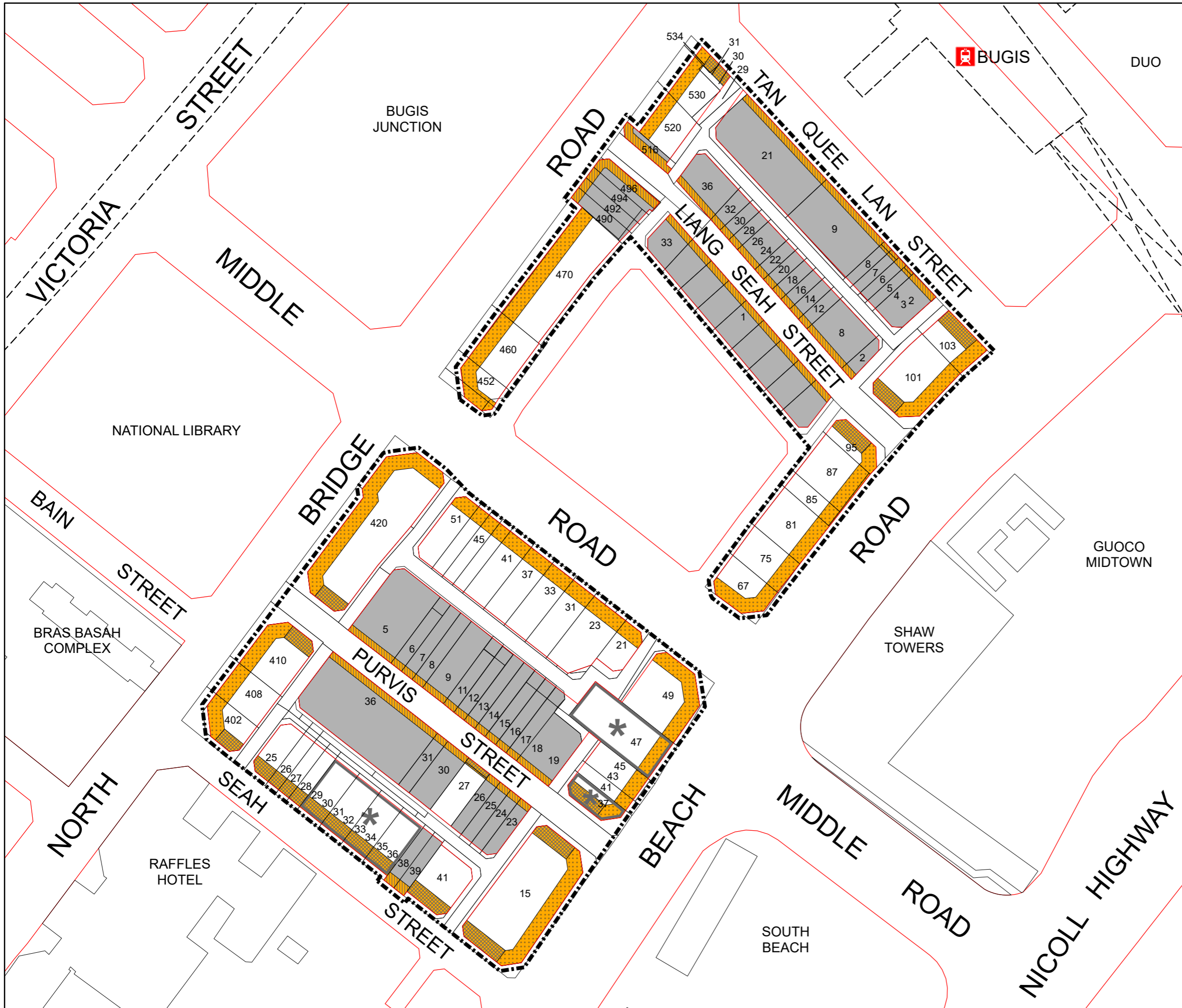
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DATE: OCT 2025

GROUP PCUDG





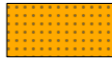





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**1ST STOREY
PEDESTRIAN NETWORK PLAN**

LEGEND

-  Boundary of Street Block Plan
-  Lines of Road Reserve
-  House Number
-  Conserved Buildings
-  Minimum 3.6m wide and 3.0m clear Covered Walkway
-  Minimum 3.0m wide and 2.4m clear Covered Walkway
-  Covered Walkway within Conserved Building
-  Site subject to Detailed Envelope Control Guidelines

Note: All differences in level along the Covered Walkway are to be mediated by ramps. Steps are only allowed for steep gradients where ramping is not possible.

SCALE : 1:1,500@ A3



DRWG NO: DT/2025/021

DATE: OCT 2025

GROUP PCUDG



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